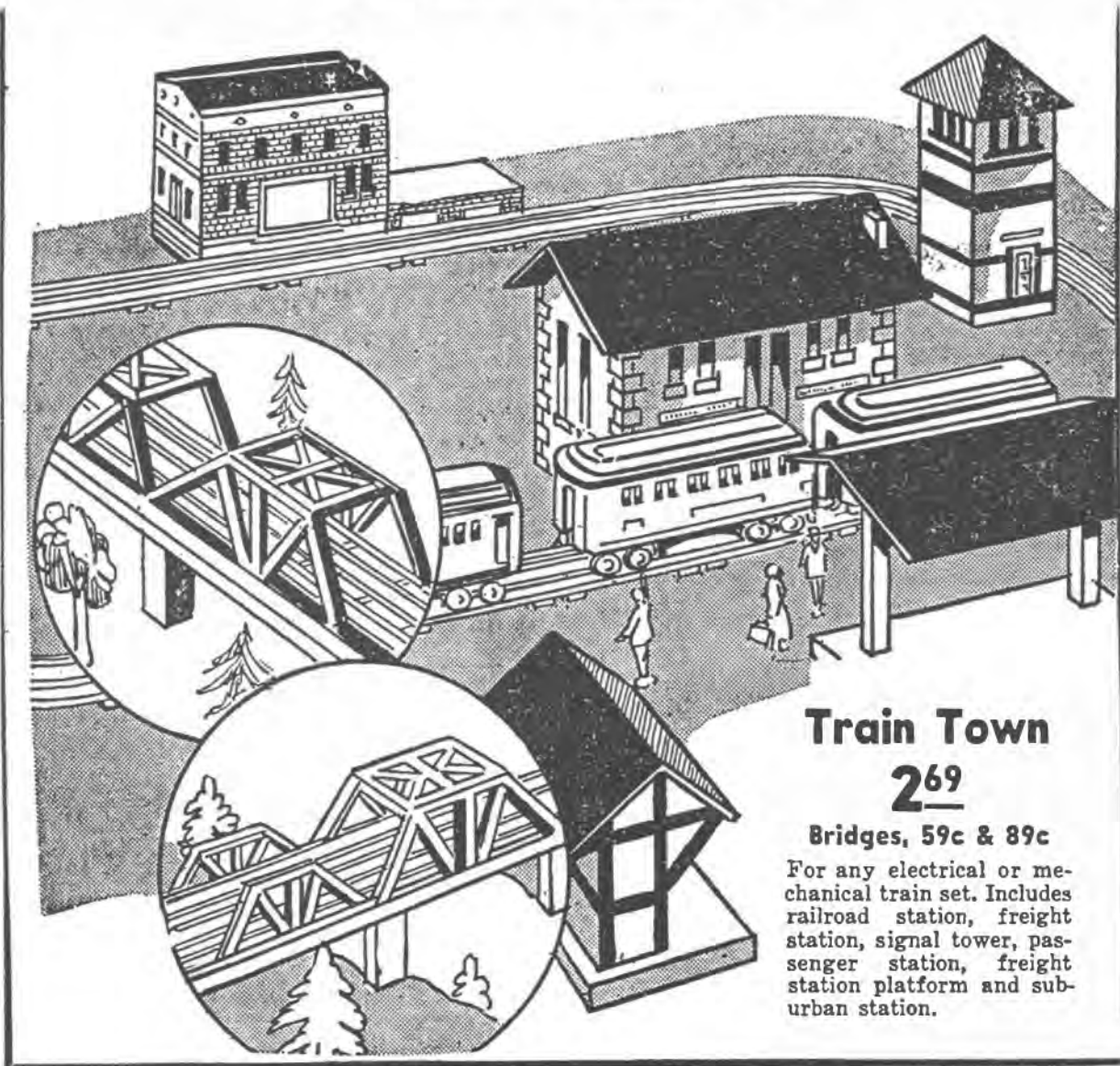


THE ATLANTIC DIVISION EXPRESS



Train Town 269

Bridges, 59c & 89c

For any electrical or mechanical train set. Includes railroad station, freight station, signal tower, passenger station, freight station platform and suburban station.



I.C.A.



WINTER 1991

TRAIN COLLECTORS ASSOCIATION

ATLANTIC DIVISION EXPRESS

Vol. XXI - # 1 - Issue 81

WINTER, 1991

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EDITOR
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ATLANTIC DIVISION MEETS - 1990 - WESTOVER C.C. - NORRISTOWN, PA.

Sunday, May 26, 1991
Sunday, July 21, 1991
Sunday, November 17, 1991
Sunday, January 19, 1992

ON THE COVER

Here we see the earliest known advertisement for the Train Town line of toy train accessories. In this issue we finish the known history of this small manufacturer. In addition to the accessories outlined in this article (and in last fall's issue of the National TCA Quarterly) Mr Satterthwait was designing and selling "Sail-Me" paper airplanes, "pop ups", which are paper figures that one could put on a toy train layout, and something that I cannot identify called "Asbestrays". I could do a small article about the first two of these items, but to call them "toy train related" is stretching a point. Dick Knowles would probably hang me by some appendage or another if I wrote such an article. Therefore I don't plan to. However, if enough of you let me know that you would be interested, I'll take my chances in respect to avoiding his grasp. Let me know your feelings.

This issue of the *Express* is an experiment. In the past an attempt has been made to try to balance the quarterly with "variety". Long articles like this one have been split into sequential parts for this reason. Please let me know your preferences. Do you prefer one long article, like this issue, or a series??

Let the Kiddies Meet Santa Claus at

Sears TOYLAND

They'll Want This

**Train
Town
\$2.69**

Bridges, 59c and 89c

For any electrical or mechanical train set. Includes railroad station, freight station, signal tower, passenger station, freight station platform and suburban station. Easy to set up... doubles the pleasure the kiddies get out of their train sets.

Doll-in-Carriage



\$2.69

Big 15-in. composition doll and real 22-in. doll buggy. Carriage robe, cab pad, dress and bonnet included.

Lincoln Highway Toy



98c

Electric lighted mechanical auto, filling station and lamps, just like real ones. Complete with batteries and bulbs.

Hundreds of Other Toy Values!

- | | |
|---------------------------------|------------------------------|
| Child's Wicker Chair... 1.00 | 24 Pc. China Tea Set... 98c |
| Playhouse and Store... 1.00 | All Steel Scooter... 2.79 |
| Cadillac Car... 10.98 | Doll Buggy... 5.98 |
| All Compo. Baby Doll... 59c | Real Hair Doll... 4.98 |
| Steel Wagon... 98c | Drum for Boys... 1.19 |
| Mickey Mouse Target Game... 98c | Chest with 15 Tools... 1.19 |
| Wagon of Blocks... 49c | 2-candle Elec. Wreath... 59c |



Fig. #1 - Sears ad - 12/8/33

TRAIN TOWN - PART III
CWW

Those of you who have been long time readers of the Atlantic Division Express may recall several articles of mine regarding "Train Town" accessories^{1,2}, as well as an article about track pads³. Both of these lines were produced by local (Pottstown, Pa.) resident, Charles S. Satterthwait. In one of John Newbraugh's "Who Done It?" series in the National TCA Quarterly I noticed that a couple pieces that had been sent for identification were "Train Town". So, last spring I sent John not only my identification of the pieces, but a whole wad of information that led to the large article that appeared in the Oct., 1990 TCA Quarterly. As John indicated there, I wrote the following article which was to appear as

a later installment. National TCA and I had a minor disagreement, so I am printing it here. It is assumed that you have a copy of this issue to refer to if you are interested. Please do so as I have not repeated what John published last fall. (John also relates the somewhat serendipitous story about how Frank Bowers got all of these original artifacts for me. This little aspect to the story is possibly more interesting than the Charlie Satterthwait saga itself!)

The front cover and figure #1 show the first newspaper advertisements to appear regarding the availability of the Train Town accessories, both from Sears, Roebuck full page advertisements that appeared in the Philadelphia Evening Bulletin. The cover is from November 30, 1933 while figure #1 is from December 8, 1933. Both of these as well as the "mats" used to make the newspaper copy were in the artifacts that Mr. Satterthwait had kept. Other than an ad from 1941 (to be shown later) I have found no other advertising for this line, although Charlie's books indicate that on October 27, '33 a set of buildings was sent to J.S. Lovingham who was editor of *Toys and Novelties* magazine at that time. I don't know if a review ever appeared in that journal. Maybe one of you who like library searches could try to find a file of this periodical and let us know if such a review ever appeared??

As implied in the Oct. TCA article, Mr. C. S. Satterthwait

started production in August, 1932 and used a printing firm in Pottstown, Pa. the name of which I have not been able to discover (yet!). The Train Town line was printed and marketed out of Pottstown until August, 1933 when the printer was changed to Brown & Bailey Co., of Philadelphia. The structure set box tops as well as all advertising paraphernalia reflect this change. As you will see below, Charlie upgraded the line by altering the structures somewhat when the printer was changed. The Aug. - Nov., 1933 sales record books indicate that the items were sold from both locations. But, the records in my possession imply that the bridges were produced only in Pottstown and only in 1932 and were then marketed until all stock was gone. Unfortunately, the four bridge boxes that I have are not marked with the company address like the structure sets are. If someone has a Train Town bridge box with a Philadelphia address, I would like to know about it.

Lets take a look at the production structures and the variations thereof. Figure #2 shows the two boxes. At the top is the "417 North Eighth St., Phila., Pa." box while that on the bottom is labelled "Hanover and South Sts., Pottstown, Penna." The illustrations on the boxes seem to be quite accurate in respect to the contents. The "Pottstown" set is the one that Mr. Satterthwait himself had kept while the "Phila." set was given to me by a local collector after my

original "Train Town" articles had appeared in the Atlantic Division Express. The first obvious difference is the box size. Although both are about 2" deep, the "Phila." box measures 8.5" x 15.75" while the "Pottstown" box is 9" x 16.375". Notice the differences in the illustrations. In particular, the roofs are shingled on the "Phila." box while the roofs are smooth on the other. Also the bases of the signal tower, freight station, freight station platform, and the railroad station trim are all like fieldstone on the "Phila." set box while the similar things are either stippled (cement or stucco ?) or brick on the "Pottstown" box.

In figure #3 we see the signal towers, "Phila." being found on the left and "Pottstown" on the right. They are quite similar to the respective box top illustrations. Notice the base differences, the two tone vs. solid colored doors, and the presence of steps on the "Phila." item. Both towers are cream and brown, but the "Pottstown" brown is darker (less yellow pigment). The window sills are grey on the "Phila." piece but brown on the "Pottstown" item. One difference that is not visible in the photograph is that the "Phila." structure has green window shades while those for the "Pottstown" structure are grey. (The window shade celluloid had become un-glued when the photo was taken.) However, note that both roofs are shingled. I am wondering (and

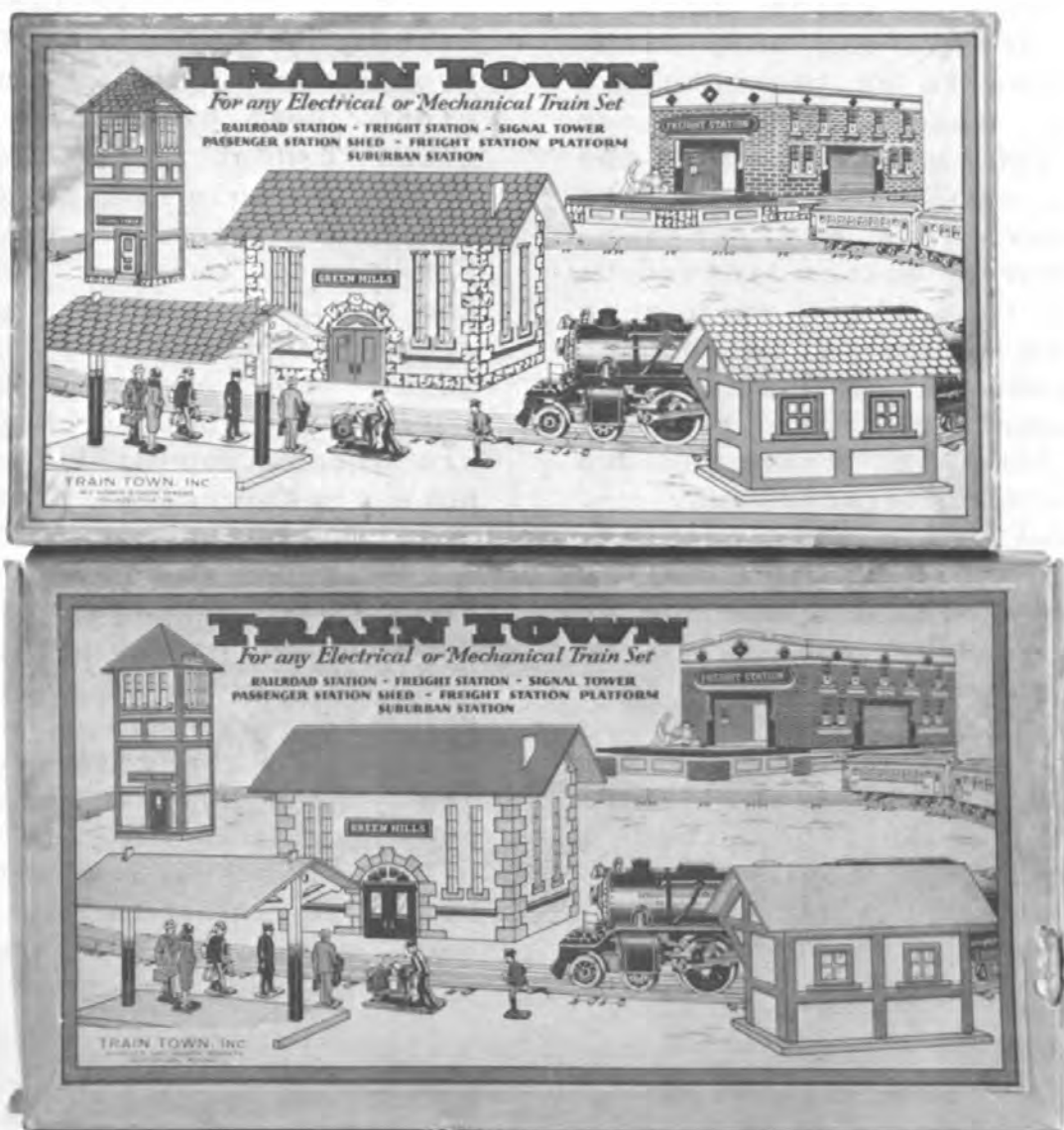


Fig. #2 - The two structure Set boxes.



Fig. #3 - The 2 signal towers.

hope one of you readers can help with the answer ?) if the item from "Pottstown" was produced as shown here or with a smooth roof as illustrated on the box top. I suspect that a smooth roof is correct and Charlie just kept this "later, Phila." roof in his artifacts for some reason. I'll comment more about this later.

Figure #4 shows the front of the passenger station. The rear and a partial view of a side can be seen in the National TCA Quarterly article. I

have another problem that I hope one of you can help with. The stations in my two sets are identical (except for some green paint slopped over the one illustrated here), yet the box illustrations are, as mentioned above, quite different. I suspect that there must be a station in someones collection that matches the "Pottstown" box illustration, and I would like to know who that someone is. I can't understand why Mr. Satterthwait kept this particular station with his "Pottstown" set. Maybe Charlie had kept one of each set and over the years his son put them away a little wrong after playing with them. If so, and his "Phila." set disappeared before I got the collection, then I would have this apparent discrepancy now.

Figure #5 shows the two production Passenger Station

Sheds, "Philadelphia" production on the left, and "Pottstown" production on the right. Both are virtually identical except for the addition of a shingle pattern to the "Phila." piece. The wooden bases are painted grey while the wooden posts were painted cream and then dipped in red paint to produce the band across the bottom half. Roofs are green. You will notice that both are identical to their respective box top illustrations.

Figure #6 shows Charlie's hand made prototypes for the Shed and the Suburban Station. Both are somewhat larger than the corresponding production pieces and the coloring is similar but not identical. The prototype differences are: (a) Mr. Satterthwait didn't bother to paint the base to the station and (b) the shed posts were dipped in green and then



Fig.#4 - The passenger station.

black paint to produce the three tone posts shown in the photo. Notice the non - shingled roofs.

Figures #7 and #8 show the front and rear views of the Suburban Station as found in the "Pottstown" set. Base and roof are two different shades of grey while the structure is printed in a creamy shade of beige with brown details. My "Phila." set is missing this piece. I assume that it would have shingles and thus match the box top illustration. However, I need one of you to confirm this. (Of course, if anyone has one to spare I would like to add one to my set !?)

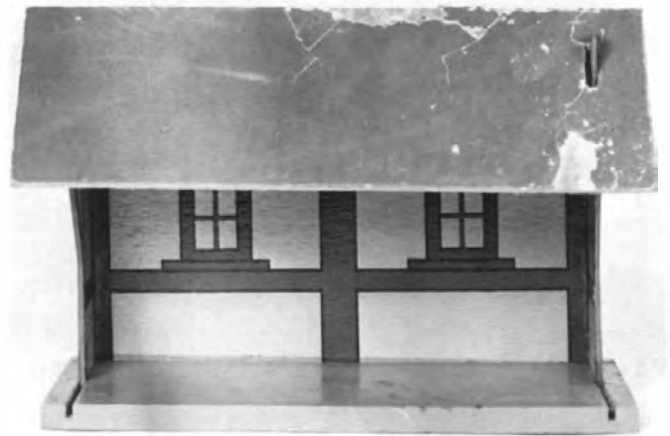


Fig. #7 - Front view, suburban station.

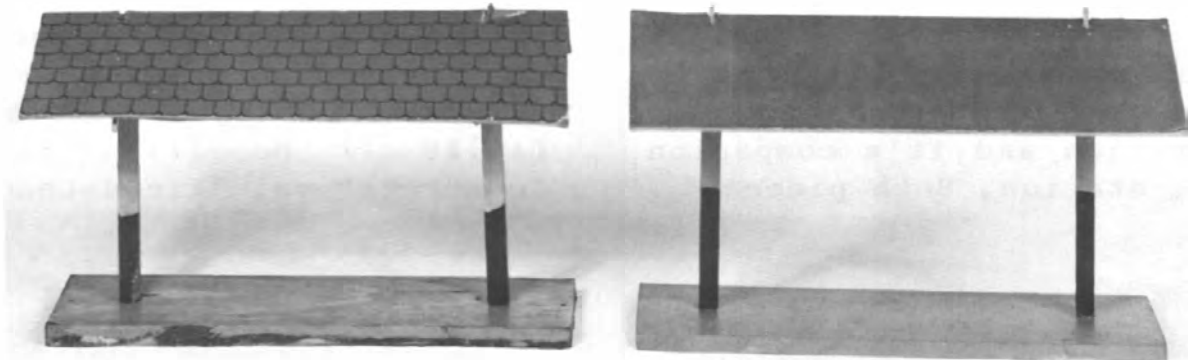


Fig.#5 - Passenger Sheds.

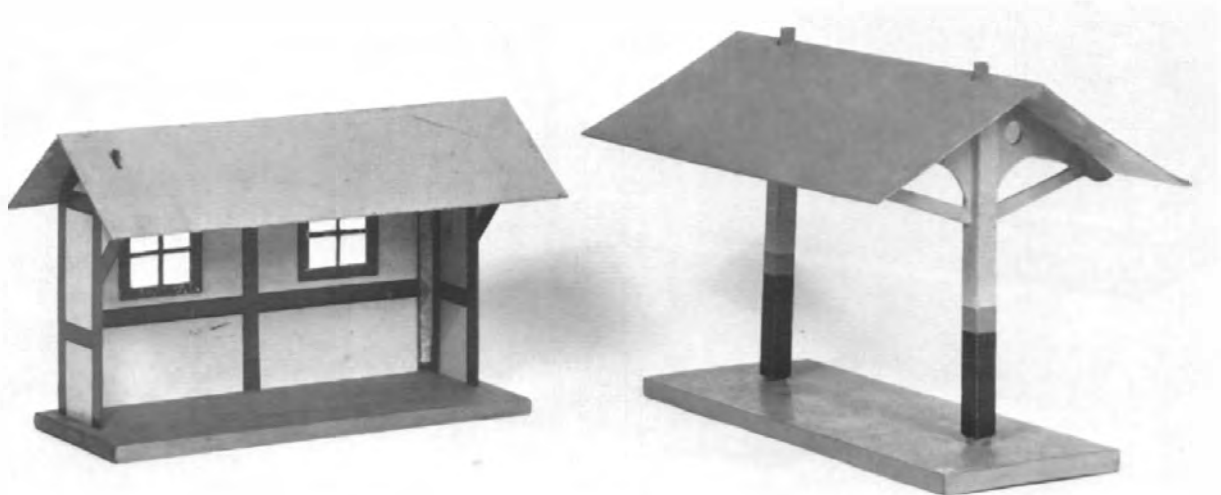


Fig. #6 - Prototypes.

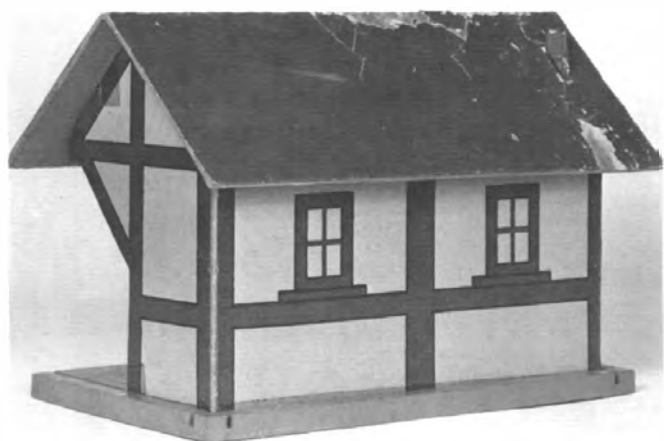


Fig. #8 - Rear view, suburban station.

In figure #9 we find the Freight Stations and the Freight Platform from the "Pottstown" set. As in the previous photos, the "Phila." item is on the left and the "Pottstown" on the right. The "Phila." production freight platform is missing but must be printed to match the box top illustration and it's companion freight station. Both pieces

have grey shingled roofs but, as we found with some of the previously discussed items, it seems likely that the "Pottstown" piece should not have shingles. On the other hand, it is also possible that Charlie had designed shingles for this item only (Pottstown production) and thus noticed the extra pizzaz that it gave the piece, so that when he altered the items for "Phila" production he decided to add this feature to the other structures also. The roofs are not visible on the box tops to determine any such information. Compared to other items in this line, the freight station received more detail changes than the others when the printing firm was changed. Visible in fig.#9 (I hope!!) is the "concrete" vs. "fieldstone"

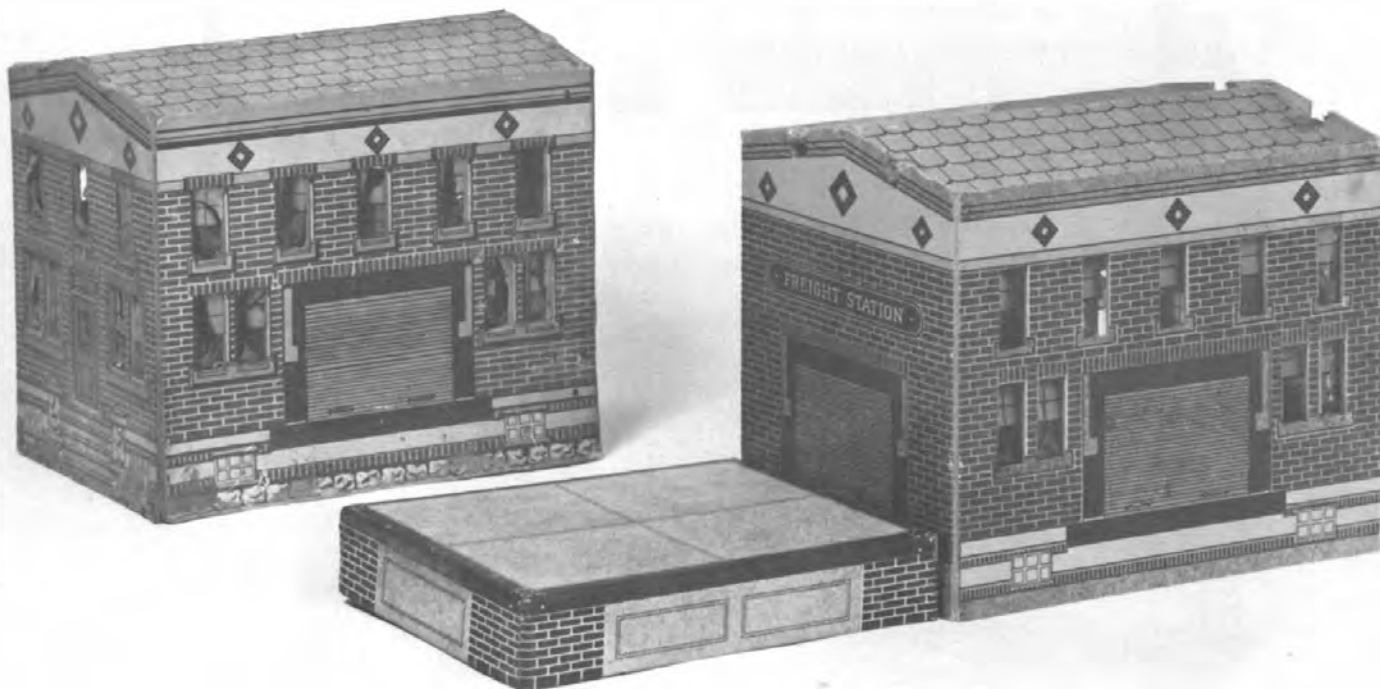


Fig. #9 - The freight stations and freight platform.

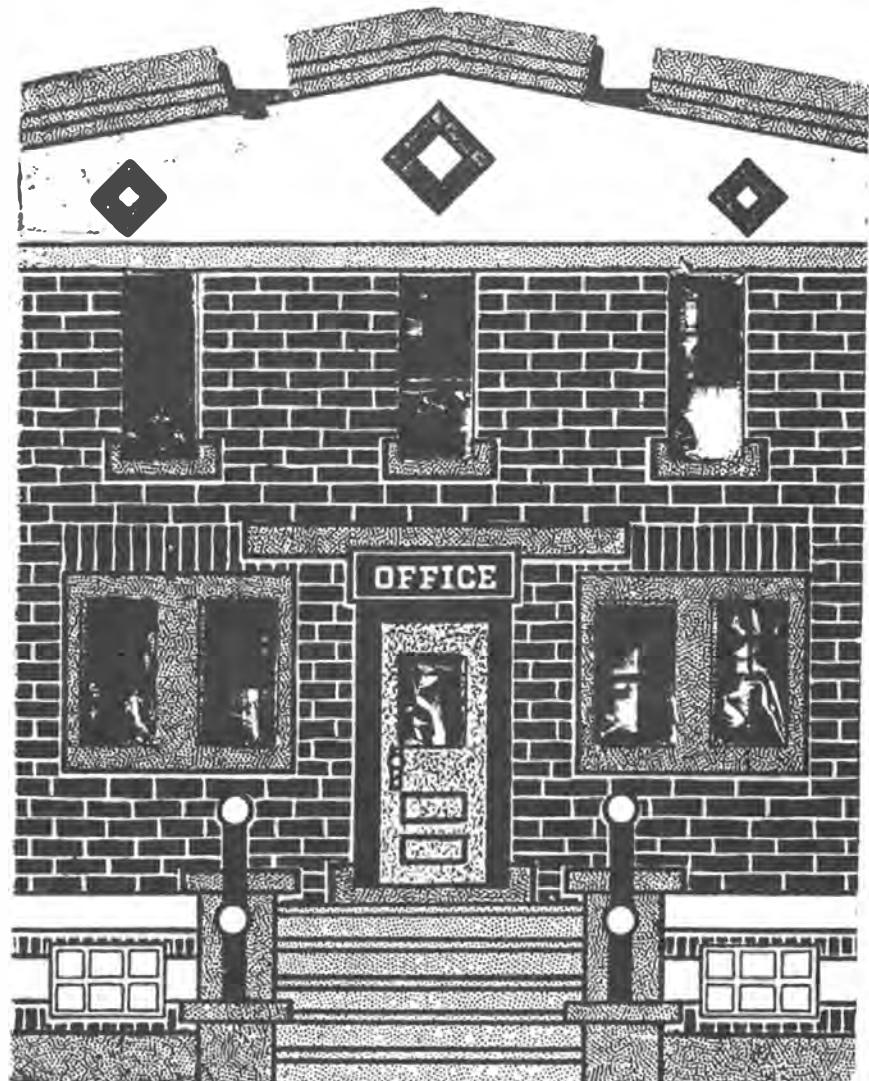


Figure #10 - Philadelphia Production

Figure #11 - Pottstown Production

OFFICE ENDS OF FREIGHT STATIONS

foundation and other details. Specifically, on the "Phila." piece there are sills and lintels on the basement windows and brick lintels have been added to the second floor windows. Both pieces have green window shades unlike the differently colored shades found in the switch towers. The office ends of the buildings are quite different. See figures #10 and #11. Comparing fig. #10 (Phila.) with #11 (Pottstown) we see (a) a fieldstone lintel over the door vs. a cement lintel, (b) fieldstone step sides with plain grey tops vs. cement step sides with simulated light posts, and (c) a totally different door design.

Although the bulk of this article presents the usual pictures and information regarding

some toy train related subject, I felt it might be of interest to some of you to see the other items which Mr. Satterthwait had made for his railroad. Whether or not Charlie ever contemplated adding any of these items to his line will probably never be known. The quality of craftsmanship is quite high compared to the home made items that I have acquired in the past. Figure #12 shows a semaphore, flag, and track crossing assembly. It should be noted that there is a mate to the track crossing that could complete the scene. I omitted it in the photograph for clarity. The track crossing base is made from an old piece of sheet metal cut and bent appropriately and painted in green, grey and various colors to sim-

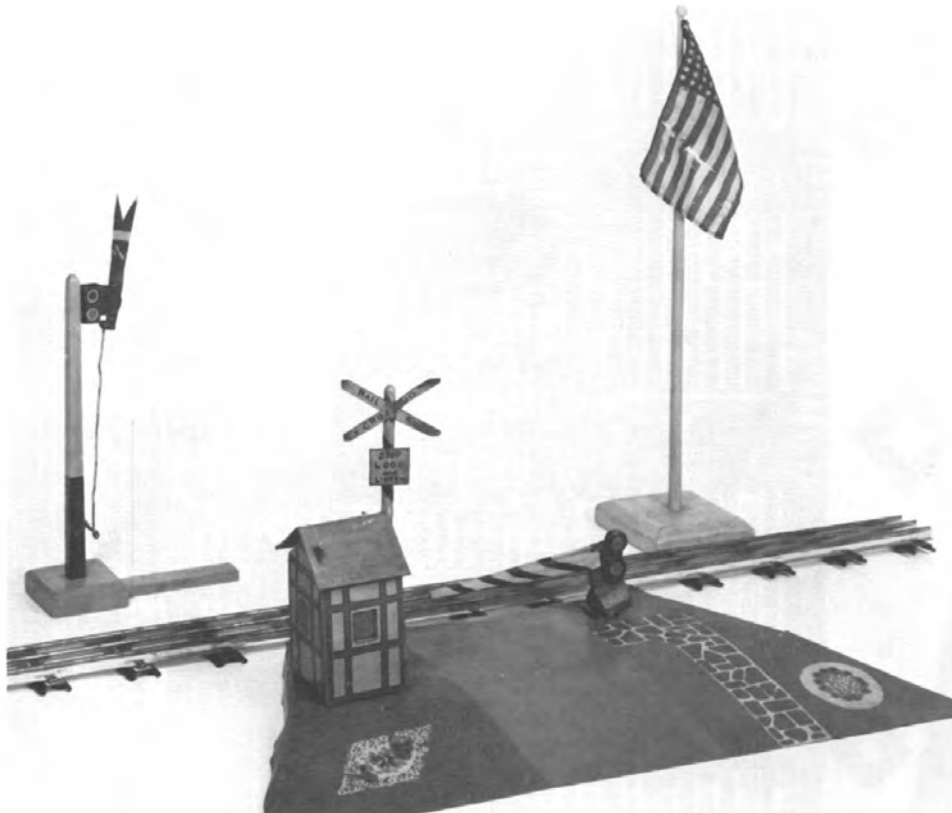
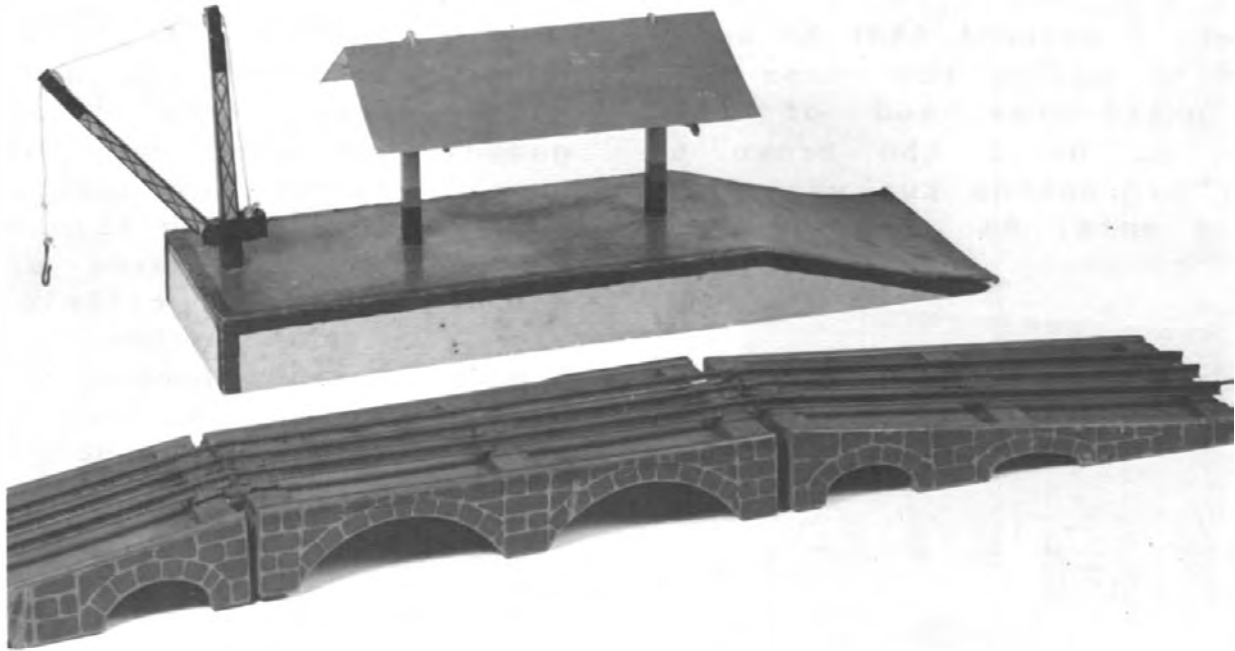


Fig. #12 - Grade crossing assy.

ulate flowers. He also put "loops" onto the top and the items to be attached have tabs to fit therein. The crossbuck assembly and gate assembly are made of assorted pieces of sheet metal and wood, as is the manually operated semaphore, while the watchman's hut is made of the same materials and

uses the same construction methods as his "prototypes" and production pieces. The flagpole is all wood.

Figures #13 and #14 show a couple different loading docks, low, three piece bridges that are similar to Lionel, et. al., and a prototype for one of his thru truss bridges. All are



Fig#13 - Loading dock & bridge.

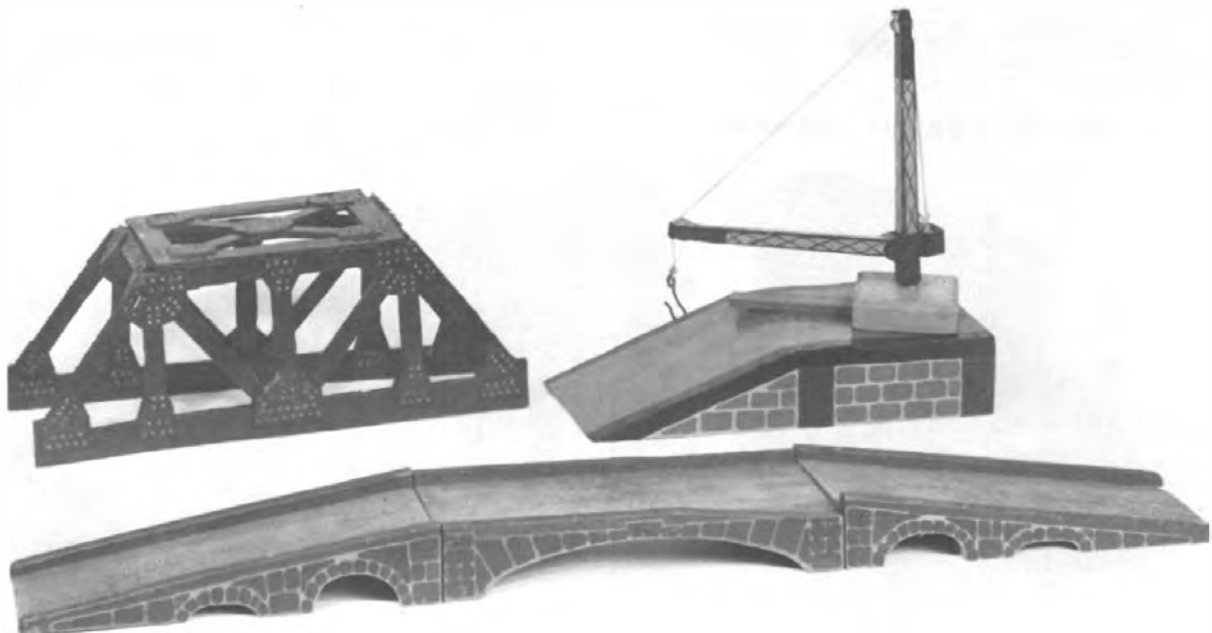


Fig. #14 - Dock, bridge, and prototype thru truss bridge.

made of wood and the thick cardboard - like stuff similar to his production items. The cranes manually operate. The prototype bridge has a truck-load of simulated rivets made of tiny nails. Rivets are not found on the production bridges.

The Satterthwait records that I obtained cease in 1935. However, I suspect that he continued to market the items either until the end of the decade or until the Brown & Bailey production run was completely sold. As you will see

below, I also suspect that he came out with a second product line just before the second world war. Of all the questions I have asked you readers in this article, this possibility is the one that I would like answered the most.

Figures #15 and #16 show four houses that are in Charlie's artifacts. Unlike the previous items, they are very nicely proportioned for 'O' gauge rather than 'Standard' gauge. They are made of the strong material as used previously, but are much thinner and are constructed using similar techniques. Clear celluloid is glued to the insides of the window openings backed by colored paper to simulate window shades and/or cut up doilies to simulate lace curtains. I think these are prototypes for his "new" 'O' gauge line. Of particular note is the house to the right of fig. #16. This house is also shown on the right of fig. #17 with the roof removed. The construction technique used in this house is unique as far as my experience

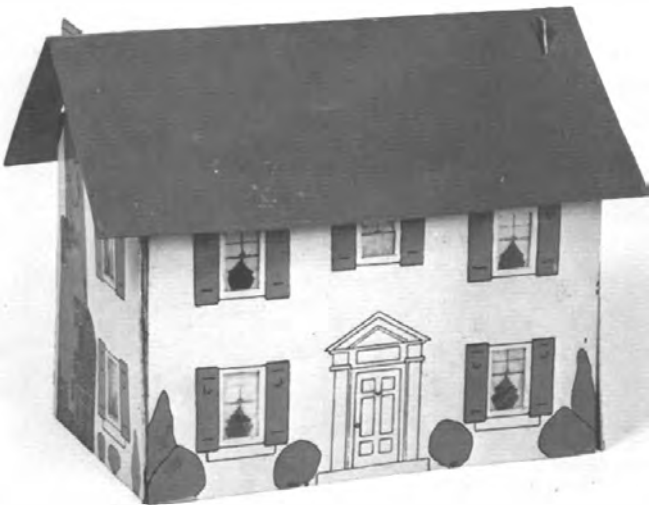


Fig.#15 - An 'O' gauge House.

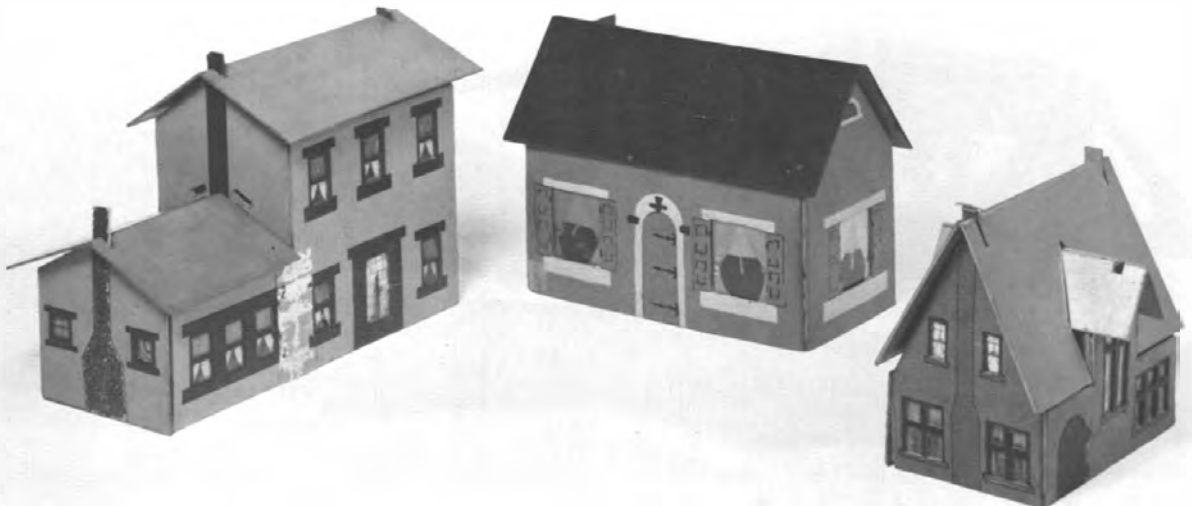


Fig.#16- More 'O' gauge houses.

is concerned in that the dormer roof goes completely through the structure and ties the two sides together. This produces considerable rigidity that would otherwise be absent. Also in fig. #17 is a cardboard production house showing this identical technique! Does anyone recognize this house as being from some other manufacturer? Or, is it a late "Train Town" product as I suspect? Figure #18 shows this house as-

sembled (except for the middle dormer roof) along with a cardboard trackside building that is also a production item from the same firm that made the house.

Other items indicate that Mr. Satterthwait at least planned a second line, this time designed for 'O' gauge. Figure #19 shows full size drawings and a mockup for a proposed airport along with two pylons, while figure #20 shows

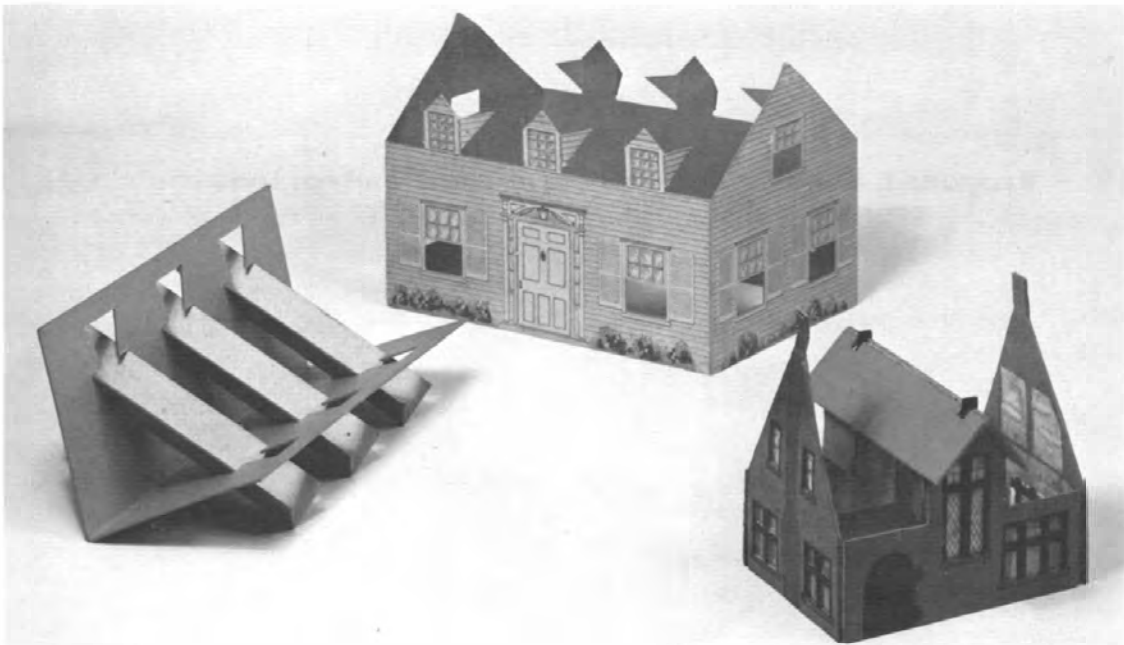


Fig. #17 - Production and home made house.

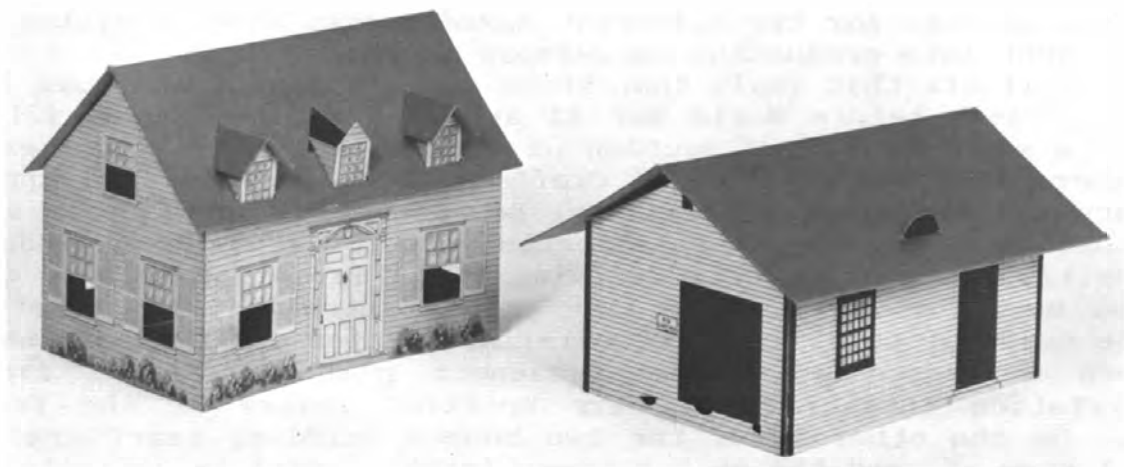


Fig. #18 - Production buildings.

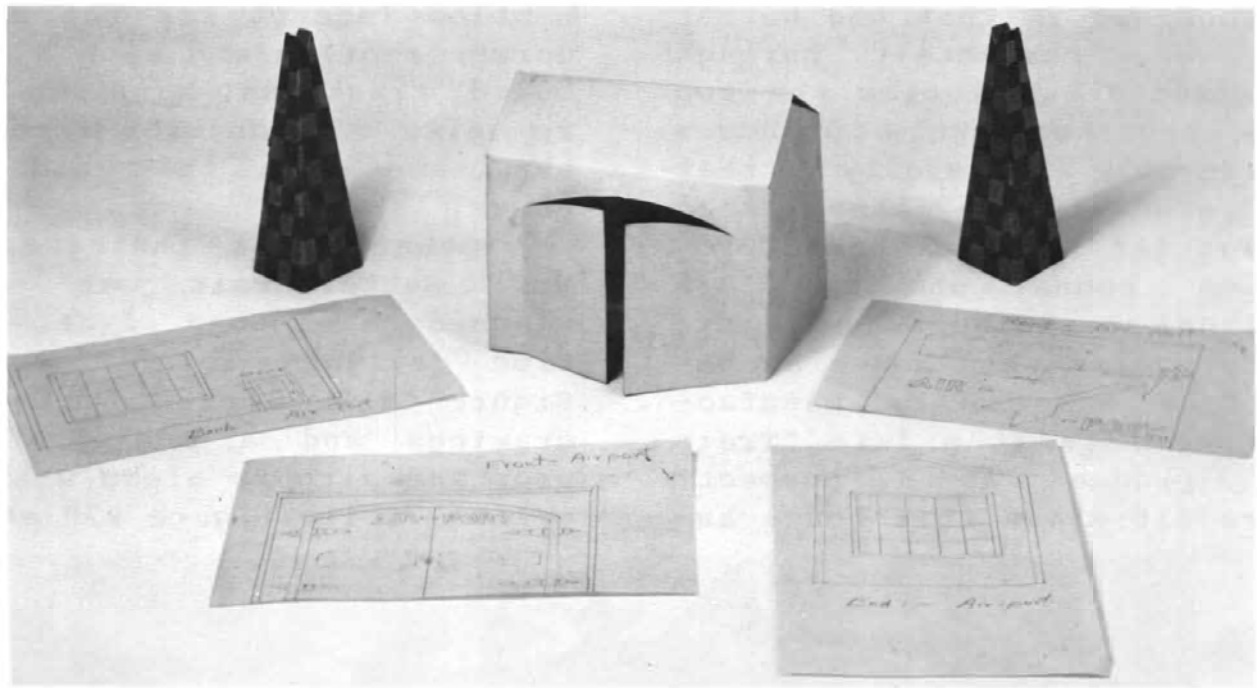


Figure #19 - Proposed hanger mockup, drawings and pylons.

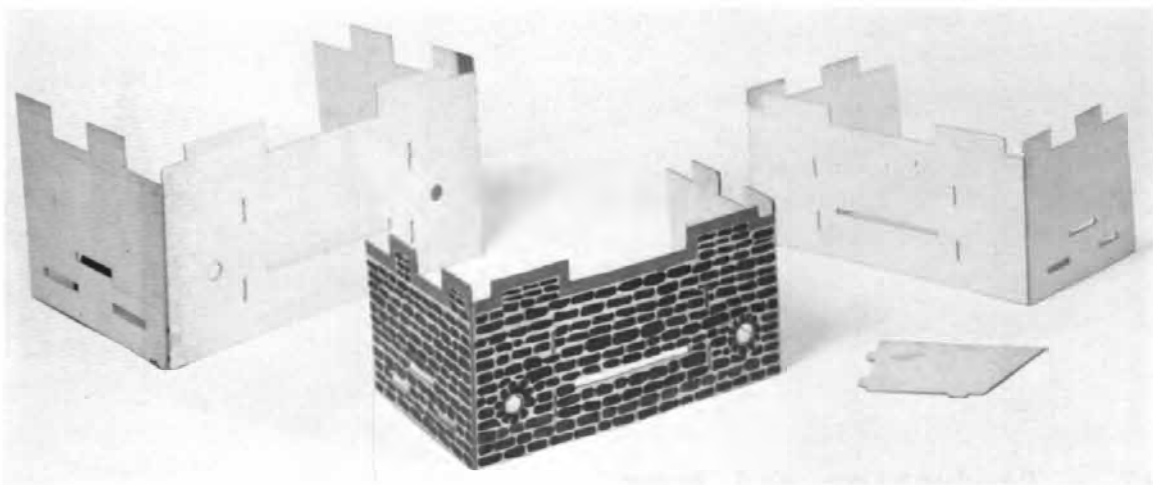


Figure #20 - Fort mock-ups. Did they ever go into production?

parts of his mockups for two different sized forts. Whether either of these items ever went into production is unknown at this time.

More artifacts that imply that there was a second Train Town line made and marketed just before World War II are as follows. Figure #21 shows a blow up of a roughly 1" x 2" section of a full page *Polk's Hobbies* ad from the December, 1941 issue of *Model Craftsman* magazine. It also appeared in the January and February, 1942 issues. Because of the small size we cannot be sure that this actually is an advertisement for Train Town products. Can anyone positively identify this building set? The box is totally different from those used for the earlier line and is somewhat reminiscent of some Built-Rite boxes of this time. The mainline station (left, rear) shows some Train Town influence while the passenger platform (left, front) and suburban station (right, front) are spittin' images of the Train Town equipment. On the other hand, the two houses (middle, rear) are not like anything I know of, and the switch tower (right, rear) is definitely fatter than the early Train Town production and very much reminds me of a Megow or Skyline product. Any help??

If the only evidence of a late Train Town line was the above, I would be less confident of its existence. However, Mr. Satterthwait kept in his artifacts the full page "STERNS" newspaper ad shown on the rear cover. This is from the December 7, 1941 *Philadelphia Record*. Unlike the early ads, Train Town is not specified in this one. Here we see an artist's conception of the six structures that Sterns was offering in this special package. They are totally unrecognizable in respect to manufacturer except for the Train Town - like suburban station and passenger platform. If the structures in this package deal are not Train Town, then why did Mr. Satterthwait keep this ad in his things? Everything in the Satterthwait collection that I bought was related to Train Town or Charlie's other enterprises.

Of subsidiary interest is the Lionel connection. Here we plainly see that the locomotive being offered is a Lionel #1684. This loco was not catalogued until 1942. How can a locomotive not catalogued until 1942 be made available on December 7, 1941? The answer is found in the TCA book.* Here we find the following listing.

1684 Black; 1689T - black; 1679 litho box - yellow/maroon, 1680 litho tank - orange Shell, 1682 litho caboose - light red/light red/cream, all cars with black journals and non-automatic box couplers. Set box marked #7008, factory promotional special shown in 1940 executive catalog. Circa 1940-41.

Thus, it appears that Sterns & Co. bought some #7008 special sets, added a dozen figures (Manoil or Barkley??) and a set of six of the new Train Town buildings and marketed it as a "49-PIECE LIONEL CITY". It would be nice to find someone who bought the whole special and still has it!

1. Atlantic Division Express, Vol. X #1, Winter, 1980, PP.6-7
2. Atlantic Division Express, Vol. XII #2, Spring, 1982, PP.10-13
3. Atlantic Division Express, Vol. XVI #3, Summer, 1986, PP.8-15
4. Lionel Trains, Standard of the World, 2nd. Ed., Train Collectors Association, 1989, P. 122



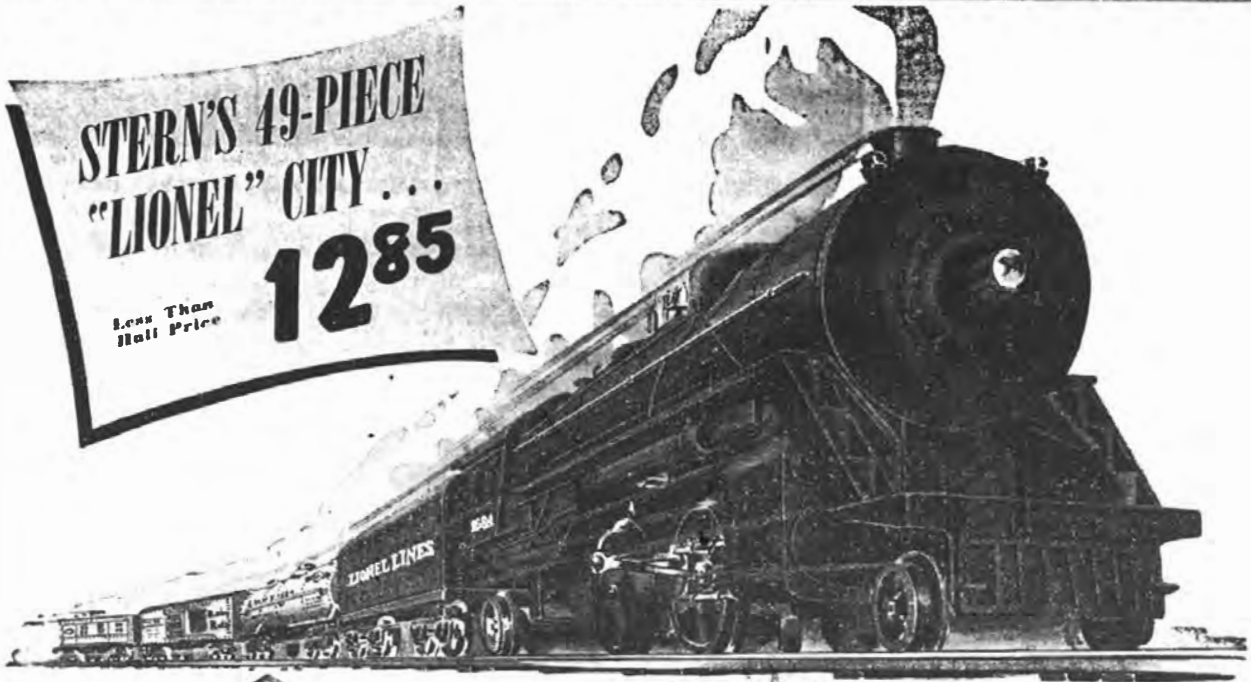
“O”
GAUGE
1/4" to
1 ft.
Buildings

Six
Kits \$1.00

Hits the top in building realism—consists of main line station, passenger platform, suburban station, switch tower, clapboard dwelling, stone house.

Fig. #21 - Blow up of Polks ad. *Model Craftsman* magazine, Dec., 1941.

**STERN'S 49-PIECE
"LIONEL" CITY . . .
Less Than
Half Price
1285**



2 SWITCHES! AUTOMATIC UNCOUPLING!

A THRILL FOR EVERY "BOY" — EXCLUSIVE WITH STERN!

Philadelphia's LIONEL HEADQUARTERS present this automatic, complete LIONEL Electric Train Outfit at less than half price you'd pay buying the pieces individually! The engine starts, stops and reverses automatically by remote control. Two switches enable you to send it whizzing around the large or small oval at will . . . past the realistic buildings and figures of passengers. And the greatest thrill of all is the remote-control automatic uncoupling! Come in! Or order yours by mail or phone! A Lionel representative is here to answer your every Lionel Train problem.

75c Down--1.50 a Month

IF ORDERING BY MAIL, SEND NO MONEY; PAY 75c ON DELIVERY

49 PIECES! YOU GET:

	Value
Automatic Reversing Engine	50.00
4-Car Train (Tender, Oil Car, Box Car and Caboose)	6.50
2 Hand-operated Switches	2.75
10 Lionel Curved Track	1.50
8 Lionel Straight Track	1.20
1 Lionel Remote Control Track	1.50
1 Lionel Transformer	2.00
12 Passenger Figures	1.00
8 Houses and Buildings	2.00
1 Lock-on Connector	.40
1 Uncoupling Control Button	.50
TOTAL VALUE	\$27.36

IF YOU CAN'T COME IN

**MAIL THIS COUPON, OR CALL
MISS MANN, LOMBARD 9800**

STERN & CO.
Send me the 49 pc. Lionel City, \$12.85:

Name _____

Address _____

City _____

I have an account Open an account No Ma

STERN & CO 706-711 MARKET ST
PHILADELPHIA
BUY OF STERN—PAY AS YOU EARN

