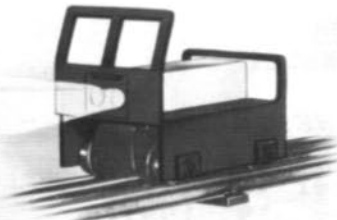


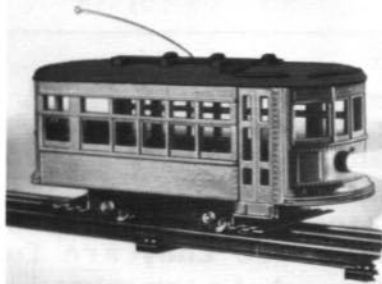
THE ATLANTIC DIVISION EXPRESS

G.M.C. ELEC-TRACTION POWERED HANDCAR

Here is the famous little GMC Handcar built to operate on three rail O-Gage tinplate track. It is die cast of Zamac for sturdy, rugged use. It is complete with a light under the seat that shines thru openings in the windshield. Simply place it on any three rail electrified O-Gage track (Lionel, Marx, Gargraves or American Flyer O) give it a push to start—and away it goes. It is powered by ELEC-TRACTION! Available at department and toy stores, and leading hobby shops. If your dealer can't supply you, you may order direct. Fully guaranteed. Completely assembled and painted, ready to run (not a kit) **\$1.95**



and STREET CAR Sensation



Brand new and filling a long felt need is the sensational GMC Street Car. It too is ELEC-TRACTION powered and designed to run on three rail tinplate O-Gage track. It is constructed in two color plastic, yellow sides and a bright red roof. Complete with operating light and lenses in the light mounting. The trolley pole on the top actually is a spring type pole. Additional realism is found with the multitude of rivets on the sides and ends. The car is 6" long, 2 1/4" wide and 2 1/2" high. It is in proportion with all O-Gage tinplate sets. Completely assembled and ready to run **\$3.95**

If your dealer can't supply you, order direct. Both the Handcar and the Street Car are shipped post paid. No C.O.D.'s.

Manufactured in America by

GENERAL MODELS CORPORATION

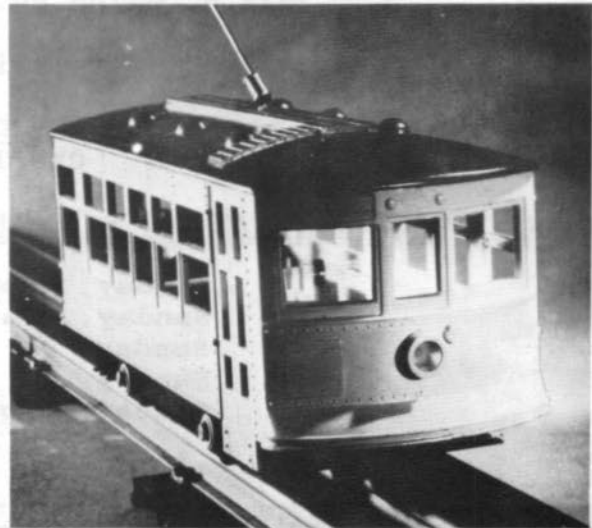
P.O. Box 66

Wheaton, Illinois

Page Fourteen

O-GAGER

GMC "ELECTRACTION" STREETCAR FOR TINPLATE OPERATION



The famous GMC Electiontraction-powered Streetcar, for operation on three-rail O gage tinplate track. Polystyrene plastic body and roof—yellow and red. Complete with front headlight and dummy spring-type trolley pole. Rivet and ventilator detail cast right on body and roof for realism. Streetcar is in proportion with O gage tinplate sets.

Simply place on any three-rail electrified O gage track (Lionel, Marx, Gargraves or American Flyer 'O'), give it a push and away it goes! Powered by ELEC-TRACTION, which utilizes front axle as rotor and copper-bakelite combined pressure-cast wheels as

brushes. No switches, no brushes to pit or wear, nothing to replace but bayonet-base 6-8v automotive bulb. Only moving parts are wheels and axles. Occasional oiling with a drop of light oil on axle bearings only maintenance required. Third-rail pick-up is slide type with very gentle spring action to eliminate excessive wear.

Available at leading department, toy, hobby stores. If your dealer cannot supply, order direct. Fully guaranteed. Completely assembled, ready to run. Not a kit. Shipped Postpaid. No C.O.D.'s

Catalog #6000 \$3.95

Page Eight

O-GAGER



FALL 1991

TRAIN COLLECTORS ASSOCIATION

ATLANTIC DIVISION EXPRESS

Vol. XXI - # 4 - Issue 84

FALL, 1991

OFFICERS

President.....William J. Wilson Treasurer.....W. Spencer Stoughton
Vice President..vacant Past A. D. Pres...Edward B. Pinsky
Secretary.....Richard D. Heineman Past Nat'l. Pres..Nicholas B. Ladd

DIRECTORS

Walter F. Ames, Jr. Al Brodhag Raymond M. Connolly George Kane
Richard H. Knowles Joe Lehman D. Garrett Spear Richard L. Stevenson

Atlantic Division - Train Collectors Association
2933 N. Wales Rd., Norristown, PA 19401

Copyright 1991

EDITOR

Charlie Weber

ATLANTIC DIVISION MEETS - 1991 - WESTOVER C.C. - NORRISTOWN, PA.

Sunday, November 17, 1991
Sunday, January 19, 1992
Sunday, May 24, 1992
Sunday, July 19, 1992
Sunday, November 15, 1992

ON THE COVER

Here we see a pair of advertisements which supplement Ed Pinsky's article found between pages 7 and 15. One of the companies mentioned in the article, GMC (General Models Corporation), published a little magazine called *The O Gager* from 2/50 through 11/50. As magazines go, these would not exactly qualify for the Pulitzer Prize in Literature, but they are full of vintage advertisements, mostly for GMC products. The photo on the left is the first ad in this magazine for the trolley (6/50, not 9/50 as indicated on page 9) and was supplemented with a cover photo of the trolley and the first announcement of it being added to the GMC line. Note that the wording next to the work type handcar implies that it was already in the GMC line at this time. This ad was repeated in the 7/50 issue. On the right of our cover is an ad from the 8/50 issue of *The O Gager* and is just a variation of the ad shown almost full size on page 14. There are actually 3 variations of this advertisement.

Thanks and kudos to the 5 folks who have sent me the material for this issue of the *A.D.Express*. In addition to Ed Pinsky, Al Brodhag and Ken McCluskey provided the Modular Layout update on pp.3-7, Robert Lakemacher sent us more info. about the Dorfan lamp (p.7), and Judy Christy sent me *The Reading Times* article on the rear cover. (Reprinted here with the permission of Reading Eagle Press.)

THE MODULAR LAYOUT; AN UPDATE

By Al Brodhag and Ken McCluskey

The modular layout and its many crew members have been busy during the *very short* but *very hot* summer of '91. The layout itself was tuned up prior to the annual trip to the Toy Train Museum -- some track-work accomplished, accessories adjusted and installed, and motive power and rolling stock readied for the trip to Strasburg. Then, from August 10th through August 17th, 2787 train fans (not counting under-fives) came through our museum and were able to actually *play with the trains*. It's one thing to look at the Museum's collection behind the glass partitions, but the parents were enthralled and children mesmerized by the sights and sounds and feel of a real, hands-on operating layout.

Kids of all ages couldn't wait to see what the next button would do -- log loaders, barrel loader, milk cars, gate-men, flashing lights, operating crossing gate, whistles, banjo signal, operating switch tower, and of course a large selection of trains (O, O27, and clock-work) -- and those who became experts left with certificates attesting to their proficiencies as Modular Layout Operators. Even though we didn't

hear of any triple busloads of crazed tourists, our crews smoothly handled steady streams of people all week. The local print media even made a visit, so look for your mugs in the Lancaster newspapers. (Ed. comment: If any of you readers can provide copies of such stories, please send same?! Thanks.)

Speaking of crews, the following Engineers, Brakemen, and Conductors served during the week, keeping the visitors informed and happy, working well with the museum staff, and seeing that our layout was fed, clothed, and bathed every day: Karl Bowers, Bill Brodhag, Bill McKay, Sr., Bill and Tim Barr, Bill Riley, Dennis and Dave Boyles, Scott Forsyth, Neal Bradley, George Doan, Junior Ames, Dick Knowles, Joe Lehman, Bob Haber, Marv Laster, Phil Ritter, John Coultres, Steven Clarke, Bob Huppman, and Ted Sowirka, Sr. and Jr. And while they don't really qualify as Engineers, Brakeman or Conductors, a tip of the oil can goes to Messrs Donze, Spear, and Stoughton for helping to pack up Saturday the 17th and for sharing a great meal afterward in the Red Caboose Dining Car. Don't believe anybody who tells you about the family with four kids who asked to be seated elsewhere when they found out that their table was just be-

hind Donze. (His friends don't call him "Windy" for no reason!!)

Atlantic Division can be proud of its Modular Layout and crews as they spread the joy of toy trains to a varied clientele. And spread more joy we will as we pin down our Fall and Winter schedule of visits. On Saturday, October 5th the Modular layout visited *Children's Seashore House* in Philadelphia. We have tentatively scheduled a visit to *Saint Christopher's Hospital for Children* for Saturday, November 23rd. Negotiations are under way for January, March, and May visits; the museum visit will probably be the second or third week of August next year, and the Layout will be at the September, 1992 "Members - Only - Meet".

Some of you are probably out there saying "Sounds like the Modular Layout is still doing the same old stuff!" How can you say that with all that's going on? We don't know, but just in case you think we should be doing more, we'd like to report that we are doing more. At the November 23rd visit to St. Christopher's, the Atlantic Division will present its first annual *Gift Layout* to the people there to use as they see fit in their recreational therapy program.

The Gift Layout is an 027 oval with siding and spur, mounted on a 48" x 77" board with folding legs. Lights, roads, buildings, accessories, scenery, and (of course) two sets of trains make this first Gift Layout a complete railroad empire. It should give many hours of relaxation and provide much therapeutic benefit to the patients. And for those of you who were wondering, yes, we have let the staff of St. Christopher's know that they will have to *closely* monitor the physicians' play time on the layout so that the trains are available occasionally for the patients. (Ed. Note: I sure hope Dr. Kates is not on this staff!!)

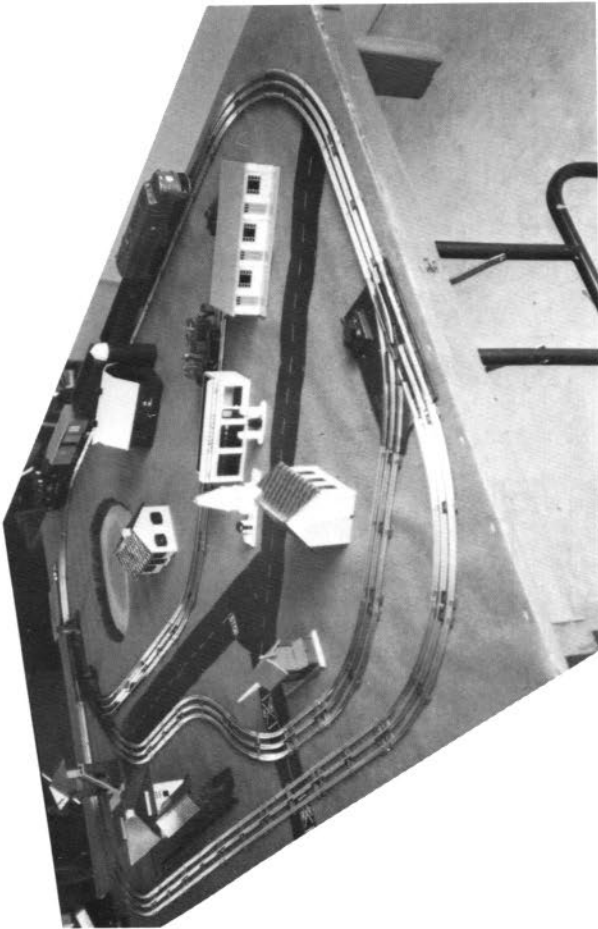
With all this activity, the Modular Layout does have some needs from you, the members of the Atlantic Division. First of all, if you would like to be on one of our crews, please call Ken or Al; we'd love to have you. It's a very rewarding experience that you won't soon forget. Second, we have some needs in the way of equipment. For the Modular Layout, we could use an operating barrel car (#3562), operating milk car (#3462 or #3472), and if you're really feeling generous and could use the tax write-off, a diesel switcher (#622, #623, or #6220). (Ed.: Keep in mind that



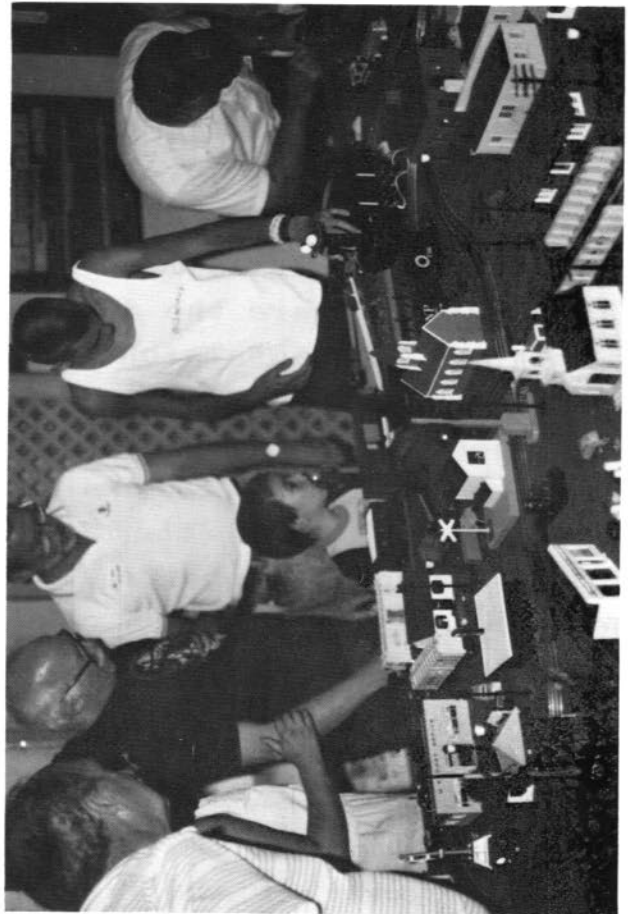
Ken McCluskey working on Gift Layout.



Above and right: Some of the people and the Modular Layout at the National Museum.



Above: The Gift Layout nears Completion



the layout is to play with and can use nicely running engines whose external appearance makes them of little value to the collector, as well as stuff that is spiffy.) For the continuing Gift Layout project we will have ongoing need for 027, manual switches, Plasticville (houses, switch towers, stations, churches, and barns), small 027 train sets (new), gang cars (#50), and any scenery that you think would be appropriate for this type of gift. Thanks for considering the Modular Layout and its many service projects as you ponder what you might be able to donate to help us out.

And thanks to all the members of the great *Atlantic Division* who support the endeavors of the Modular Layout. This project is unique in all of TCA as it seeks to *share* the joy of toy trains with those around us who are less fortunate or simply in need of a fun day. Atlantic Division members can be inwardly proud that they are the division that supports this unselfish, caring outreach of our great hobby.



More About a Streetlight

On the cover of the Winter, 1989 issue of the *A.D. Express* was a streetlight that was identified

as a Dorfan. In the Summer, 1991 issue of the *Express* a reply from Al McDuffie was printed wherein he indicated that he did not think it was a Dorfan. Here is another reply, this time from Robert Lakemacher of Powell, Ohio.

"Enclosed is a photocopy of a 1930 Dorfan catalog page..." "The catalog illustration is an artist's rendering rather than a photograph; it again demonstrates why catalog art is not always reliable but must be thought of as art. However it seems that the photograph (in the two *Expresses*) is of a fairly rare Dorfan lamp post #431 (110 volts) or a #433 (used with a transformer)."



LAMP POST No. 431

A very realistic lamp post. Artistic design. Very solid construction. This lamp post can be used with 110 volts, either D.C. or A.C. It is especially recommended for display purposes or wide gauge railroad outfits. 13½ inches high. Shipping weight, 16 oz.

Retail price, each **\$3.00**

LAMP POST No. 433

Same as above, but for 14 volts to be hooked up on the transformer. Shipping weight, 16 oz.

Retail price, each **\$2.50**

E M C O - H I C O - G. M. C.

By Edward B. Pinsky

Here we have another "family affair" among toy train manufacturers. At least EMCO and HICO appear to be directly related; how G.M.C. fits into the picture is not too clear. The three names involved are:

E M C O

Ed Miller Company
New York, NY

H I C O

Hetherington, Inc.
Sharon Hill, PA

G. M. C.

General Models Corp.
West Chicago, IL

TROLLEYS

Let's look at the Birney style Trolleys first. The all-plastic superstructures are identical for both the EMCO model and the HICO model. Only the wheel spacings are different: the EMCO model is for 3-rail 0 ga, while the HICO model is for 2-rail S ga. No color variations either: both have yellow bodies, red roofs and the designation "Dinkyville" in red lettering on the car sides.

What is the relationship between the two names? Perhaps the EMCO designation was used for 0 ga items in the line and HICO for S ga items. Or maybe the two names represented different periods of manufacture, multiple layers of corporate structure, geographically separated facilities, or simply changes in ownership. Dealer catalogs from the period list both 0 and S ga models under the EMCO name, but as shown in the accompanying photographs, boxes for the S ga models identify the maker as HICO. Perhaps the dealer catalogs were inaccurate or possibly HICO came into existence after 1953, the last year for which a catalog advertisement was found.

And here is where the GENERAL MODELS CORPORATION trolley enters the picture. The body of this 3-rail 0 ga car is the very same one used for the EMCO/HICO trolleys. Colors are identical, too - yellow body and red roof, but no lettering on the car sides. Did one company sell the bodies to the other(s), or were the molds acquired one from the other, or did yet another (unknown) plastic manufacturer supply bodies to all of them?

EMCO - HICO - G. M. C.

(continued)

HANDCARS

Examination of the EMCO and HICO Handcars reveals that construction and size of the all-plastic, animated cars are identical. Again, only the wheel spacings are different for the respective gauges. Both the EMCO 0 ga model and the HICO S ga model came in at least two color variations: blue handcar with yellow men and yellow handcar with blue men. Quite likely there are all blue and all yellow models out there somewhere. The color of the "Dinkyville" lettering can be blue or yellow or red, as indicated in the accompanying chart.

The gray boxes are identical in size, construction and format. Only the wording varies to indicate either EMCO or HICO information. Interestingly, the EMCO box shows the name "Dinkyville & Western" but the handcar itself bears only "Dinkyville." Is there a car with "Dinkyville & Western" lettering also?

The G.M.C. "Mac" Handcar is the odd-ball of the lot - more like a "maintenance car" and all metal with no plastic parts. Member Hilly Lazarus also has a trailing DUMMY "Mac" handcar (not pictured).

MOTORS AND HEADLIGHTS

A significant difference is found in the motors - EMCO/HICO used a 3-pole armature in its AC/DC motor, while G.M.C. used a less expensive 2-segment armature. Cars powered by this latter type of motor must be pushed to start. One other difference - both the G.M.C. trolley and handcar have operating headlights; the EMCO/HICO items are not illuminated. At least none of the EMCO or HICO trolleys studied have headlights. But all the advertisements seen indicate that both the 0 and S ga trolleys had operating headlights. Were both illuminated and non-illuminated versions produced?

All the cars described are 4-wheeled and run in one direction only. They were marketed in the early 1950's as evidenced by some typical advertisements of that period:

1950	G.M.C.	0 ga	Trolley	\$3.95	1953	EMCO	0 ga	Trolley	\$5.95
1952	EMCO	0 ga	Handcar	\$5.00	1953	EMCO	S ga	Trolley	\$5.95

EMCO - HICO - G.M.C.

(continued)

Can we pinpoint all the dates of manufacture? Were there other items in the various lines? Does anyone have any information on these little-known companies?

ITEM	MFG.	CAT.#	GA.	LENGTH	ILLUM.	MEN	CAR	ROOF	LETTERING	MARKING
Trolley	HICO	S-520	S	5 3/4"	*	-	Yellow	Red	Red	Dinkyville
Trolley	EMCO	0-500	0	5 3/4"	*	-	Yellow	Red	Red	Dinkyville
Trolley	G.M.C.	6000	0	5 3/4"	Yes	-	Yellow	Red	-	None
Handcar	HICO	S-530	S	5 1/4"	No	Blue	Yellow	-	Blue	Dinkyville
Handcar	HICO	S-530	S	5 1/4"	No	Yellow	Blue	-	Yellow	Dinkyville
Handcar	EMCO	0-510	0	5 1/4"	No	Blue	Yellow	-	Red	Dinkyville
Handcar	EMCO	0-510	0	5 1/4"	No	Yellow	Blue	-	Yellow	Dinkyville
Handcar	G.M.C.	?	0	2 3/4"	Yes	-	Red/Yel	-	-	None

* Possibly - see text.

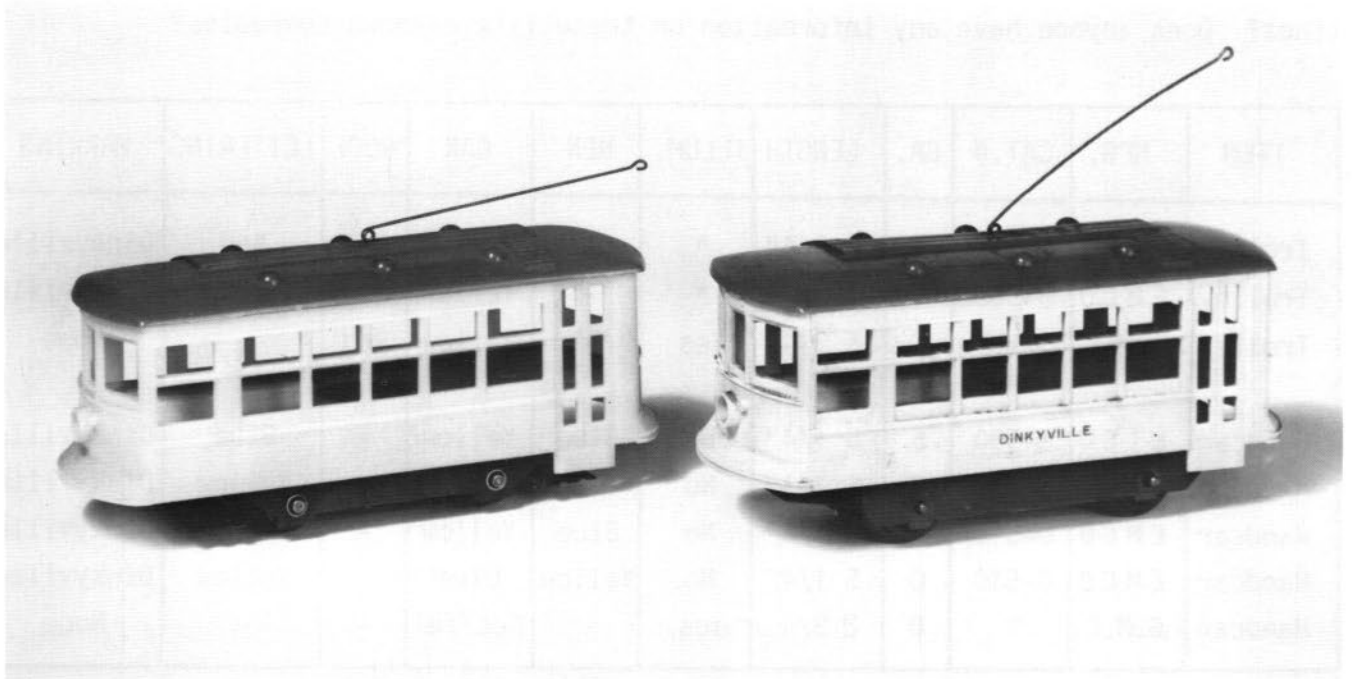
After the above was written, member Dick Stafford found an interesting article by Reynolds Galbraith which appeared in TRACTION & MODELS for July, 1965. The following excerpt from that article is reprinted here with the kind permission of Vane A. Jones, long-time publisher of TRACTION & MODELS.

"General Models Corporation, although a manufacturer of scale model [trains], also produced a four-wheel 0 gauge Birney for tinsplate three-rail use. These were first advertised in August, 1950. After the sale of this Company, the cars were subsequently marketed by the KROLL CAR COMPANY, and lastly by EMCO (ED MILLER COMPANY). As a KROLL car, it was issued as a kit. As an EMCO, it was sold [RTR] either for 0 gauge three-rail or S gauge two-rail operation."

This clears up some of the mystery, but doesn't mention HICO. It also poses a new question, namely, what do we know about the KROLL CAR COMPANY?

E M C O - H I C O - G . M . C .

(Continued)



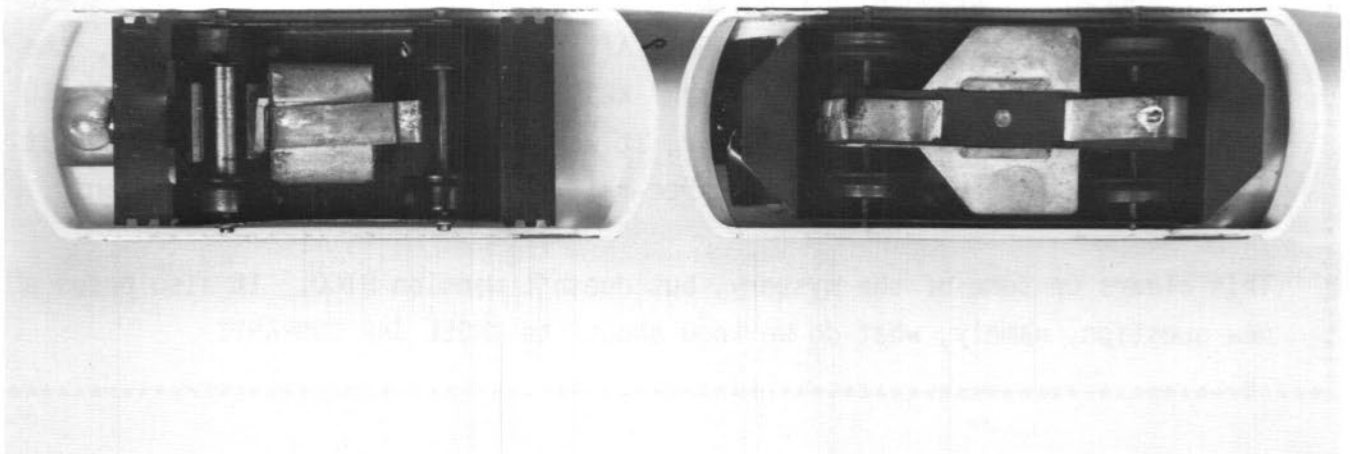
G.M.C. O GAUGE

#6000

TROLLEYS

EMCO O GAUGE

#0-500



E M C O - H I C O - G . M . C .

(Continued)



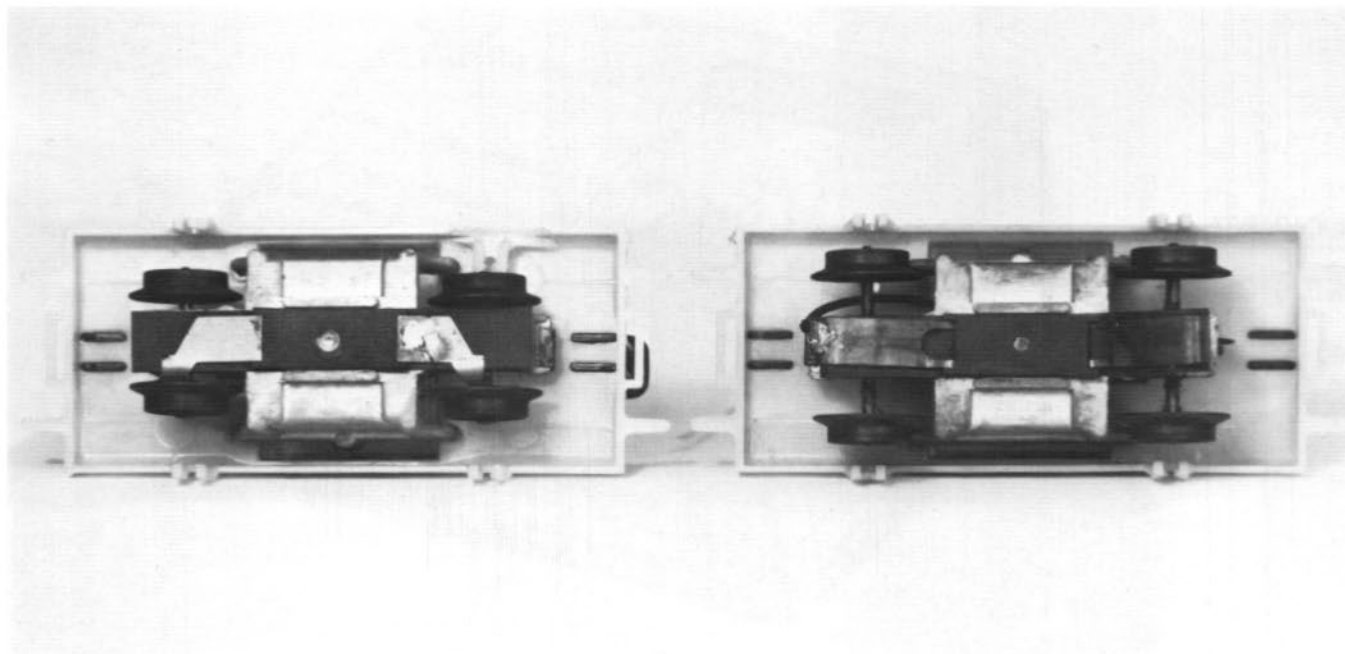
HICO S GAUGE

#S-530

HANDCARS

EMCO O GAUGE

#O-510



E M C O - H I C O - G . M . C .

(Continued)



HANDCAR BOX

HICO S GA

#S-530



HANDCAR BOX

EMCO O GA

#O-510

E M C O - H I C O - G . M . C .

(Continued)



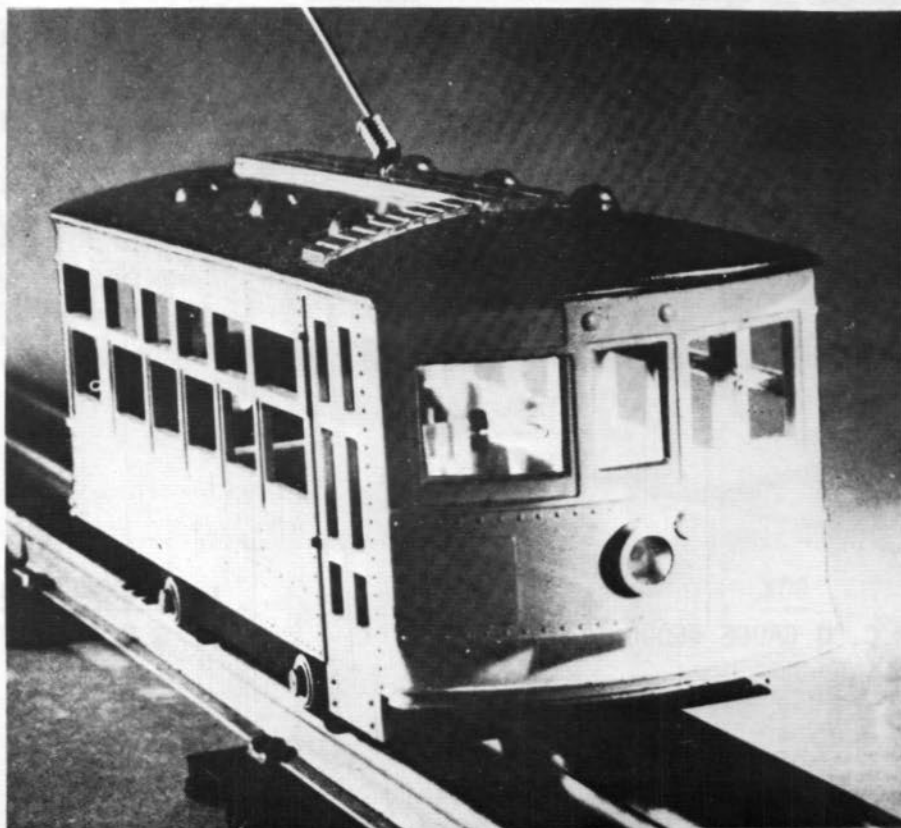
BOX
 G.M.C. 0 GAUGE #6000 ↑

↓ HANDCAR AND BOX
 G.M.C. 0 GAUGE



E M C O - H I C O - G . M . C .

(Continued)

for your tinplate pike.....

The nicest little streetcar you could want

It was the sensation of the New York Toy Fair. A piece of equipment everyone's always wanted and at a price everyone can afford.

The G M C Electraction Street Car operates on any three-rail O gage track, Lionel, Marx, Gargraves or American Flyer. Place it on the electrified track, give it a push to start, and away it goes!

Body and roof of red and yellow polystyrene plastic with rivet and ventilator detail cast right in for realism. Street car

Page Two

is in proportion with O gage tinplate sets. Powerful headlight to show the way. Only two moving parts, requiring minimum of maintenance.

No O gage tinplate system is complete without at least one of these realistic cars. For your urban sites, use to add effective color and realism.

Fully guaranteed. Completely assembled ready to run. Not a kit. Shipped post-paid. No C.O.D.'s. Catalog #6000 \$3.95

O.GAGER

ADVERTISEMENT FROM "G.M.C. O-GAGER" (OCTOBER 1950)

EMCO - HICO - G. M. C.

(Concluded)

EMCO PRODUCTS

0-500 TROLLEY 0-510 HANDCAR
FOR O GAUGE, THREE-RAIL, ELECTRIC TRAIN TRACKS

INSTRUCTIONS

This EMCO product is powered by a specially designed, real miniature electric motor that operates on AC/DC current of 8 to 18 volts range. For best results use the 5 to 16 volt variable current source on your transformer. **DO NOT OPERATE ON CURRENT EXCEEDING 18 VOLTS.**

Bearing surfaces should be lubricated with a *light, non-gumming* machine oil . . . **SPARINGLY.** Put **ONE** drop of oil only, on motor bearings, wheel bearings, gears and on rail contact shaft and bearing (pick-up shoe). **DO NOT GET OIL ON RUNNING SURFACES OF WHEELS. DO NOT GET OIL ON ARMATURE, COMMUTATOR OR BRUSHES.**

BEFORE OPERATING, be sure your track is clean. Be sure all sections of track are pulled tightly together at joints for smooth operation and good electrical contact. Be sure your track is not short-circuited . . . that the leads from your transformer are connected properly.

Put the unit on the track, turn the power on the transformer up slowly and away it goes. The bearings and gears will be tight when first operated . . . so run the unit slowly at first to "run in" the moving parts . . . just as you would a new car.

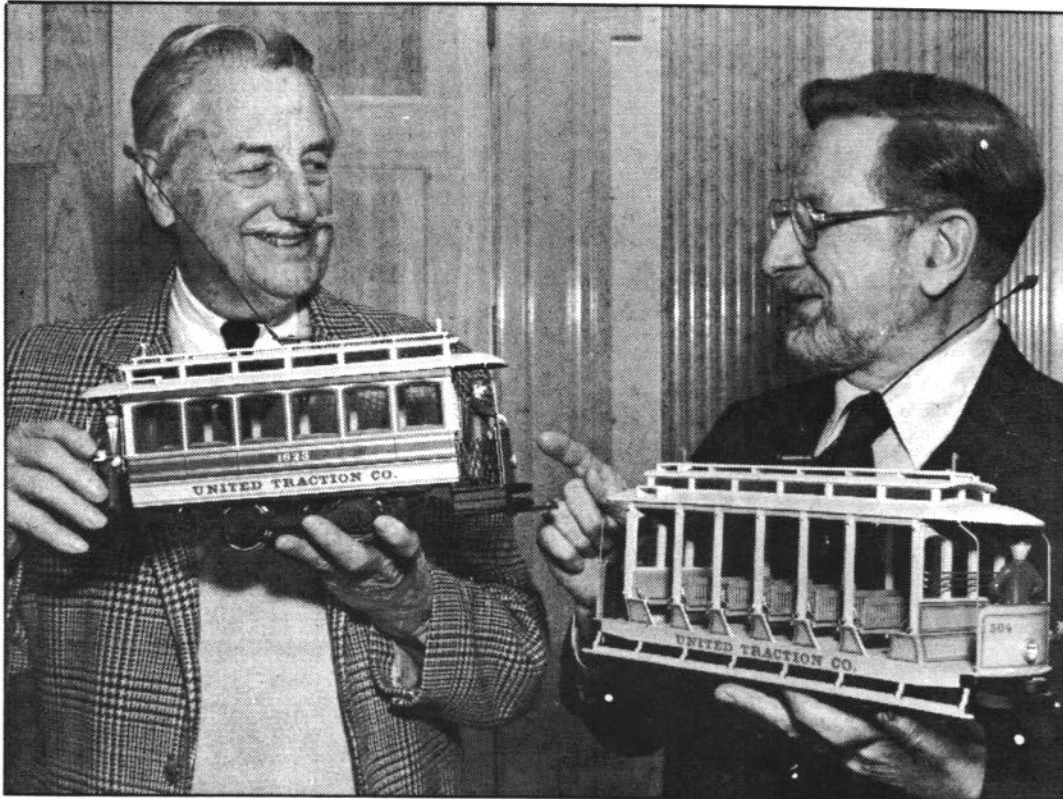
This unit was checked several times during the process of assembly . . . the motor was tested thoroughly and the completed unit was test run on a 50 foot length of track before being boxed. **DO NOT TAMPER WITH THE POWER UNIT. DO NOT TAKE THE POWER UNIT APART. IF YOU DISTURB THE SETTING OF THE BRUSHES THE MOTOR MAY NOT RUN UNTIL THE BRUSHES HAVE BEEN RE-ADJUSTED PROPERLY.**

After the motor has run for a long time the commutator may need cleaning with fine sandpaper and the slots between the segments of the commutator may need to be cleared. For cleaning, adjustments and repairs, see your local electric train service station.

***With Proper Care This EMCO Product Will Give You
Many Hours of Fun and Good Service.***

EMCO PRODUCTS 200 FIFTH AVENUE • NEW YORK 10
Factory: Richardson Park, Wilmington, Delaware

EMCO INSTRUCTION SHEET (COURTESY OF CHARLES W. WEBER)



Eagle/Times: Bud Reber

Dr. John C. Stolz, left, Berks County Historical Society council member, and Frank R. Bryson, president of the Reading-Berks Historic Transportation Association, discuss the fine points of trolley cars after a video presentation by the

association Sunday at the historical society. The purpose of the presentation was to promote the creation of a Berks transportation museum at a site with workable rail lines.

Site sought for homeless trains

By Stephanie Caltagirone

Eagle/Times

The locomotives, freight cars and cabooses owned by the Reading Co. Technical & Historical Society sit on railroad tracks in Leesport exposed to the elements.

The trolley cars of the East Penn Valley Traction Co. fare only a little better. They are scattered across Berks County and stored in seclusion.

The Reading-Berks Historic Transportation Association is trying, with the aid of a promotional video, to find or build a permanent home for the large pieces of transportation history where all can see and enjoy them.

Frank R. Bryson, association president, hopes the video — created for the group by BerksCable with funds from the state Department of Community Affairs — will prompt a speedy solution to the problem of homelessness the railroad equipment faces.

"The association was created in 1987 with the purpose of developing a museum facility for the Reading Co. and the East Penn trolley collection," Bryson explained Sunday. "The association was made for the purpose of enlightening the community on the project."

The association unveiled the video Sunday during a special presentation at the Berks County Historical Society, Centre Avenue and Spring Street.

A display of models of a future transportation museum, footage of trolleys that traversed Reading in the 1950s and a brief video history of the Reading

Co. Society preceded the association video.

Steve Gilbert, society museum committee chairman, said the society owns five locomotives, more than 20 other train pieces and hundreds of railroad artifacts, many of which have been repaired and restored by volunteer society members.

Some of the pieces had been housed at the former Reading Co. locomotive shops at Sixth and Amity streets, but the buildings were sold, and the association had to vacate.

Gilbert explained that the society is trying to organize a library that would be open for public use, but the society and East Penn need a museum in which to make this feasible.

Bryson said East Penn also needs a home for its collection of trolley memorabilia and the bodies of cars the traction company wishes to restore to working order.

The company then hopes to run the trolleys on an electrified line between Kutztown and Topton.

"We hope the community will rally behind our efforts," Bryson said.